

ROAD DESIGN PROCEDURES AND STANDARDS

The Moray Council Transportation Service Requirements for Small Developments in the Countryside

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GLOSSARY

| Reference | Description |
|--|---|
| Access Lay-By | Area running parallel to the carriageway to enable vehicles servicing properties (i.e. post/delivery/refuse collection vehicles etc) to stop/wait and avoid disrupting or blocking traffic on the road. These areas are different from passing places and are not maintained by the roads authority. |
| Carriageway | The part of a road on which vehicles travel. |
| Countryside | Any part of Moray that is beyond settlement boundaries as defined in the Moray Local Development Plan 2015. |
| Demand Responsive Transport (DRT) – | A transport service with no fixed routes or schedule that operates according to user demand and needs by a system of pre-booked journeys. |
| Developer Contributions | Developer Contributions (obligations) can take the form of either physical works or financial contributions and are used to either regulate development and the use of land or to mitigate against the impact of development. They are obtained through planning condition or legal agreement. |
| Inter visibility | This relates to the requirement to be able see one passing place from the preceding passing place and also the road in between. |
| List of Public Roads | A list held by every roads authority, identifying those roads that are managed and maintained by the authority. Moray Council holds a list of local public roads in Moray. |
| National Roads Development Guide | These are the procedures and standards for constructing and adopting roads on to the list of public roads. There are local variations for Moray. http://localapps.pkc.gov.uk/internet/flashmag/councils/nationalroadsguide/roadsfeb2014.pdf |
| Moray Local Development Plan 2015 | The Moray Local Development Plan 2015 sets the strategic direction for development strategy for Moray. It contains detailed policies and proposals used in the determination of planning applications. |
| Passing Place | A section of carriageway that is wide enough to enable vehicles to pass each other on a single track road. It is part of the adopted road and is marked by specified road signs. |
| Private Access | An access to land or property that is privately owned and maintained. There is no public right of access. |
| Private Road | Any road other than a public road. A private road is maintained by person/organisation other than the roads authority (see road). |
| Public Road | A road over which there is a public right of passage and is maintained by the roads authority (see road and list of public roads). |
| Road | As defined in the Roads (Scotland) Act 1984 S151(1) - any way (other than a waterway) over which there is a public right of passage (by whatever means) and includes the road's verge and any bridge (whether permanent or temporary) over which or tunnel through which, the road passes; and any reference to a road includes apart thereof |

| Reference | Description |
|---|--|
| Road Opening Permit | Permit required prior to undertaking work on any road (issued by the Moray Council for local public roads). This can include the formation of an access, access lay-by or passing place. |
| Roads Authority | Moray Council is the roads authority for local public roads only. The powers of the roads authority are set out in the Roads (Scotland) Act 1984. |
| Roads (Scotland) Act 1984 | Primary legislation that vests powers to the roads authority to perform their function. |
| Single Track Road | A road that is not wide enough in most places to allow vehicles to pass one another. Passing places enable vehicles to pass. |
| Stopping Sight Distance (SSD) | The distance that is required for a vehicle driver to react and stop a vehicle when faced with an unexpected obstruction in the road. This distance depends on vehicle speed. |
| Sustainable Drainage System (SUDS) | A sequence of water management practices (e.g. pollution prevention, water harvesting) and facilities (e.g. permeable surfaces, wetlands, ponds) designed to drain surface water in a manner that will provide a more sustainable approach than what has been the conventional practice of routing run-off through a pipe to a watercourse. |
| Visibility Splay | Visibility splays relate to the visibility available to a driver at or approaching a junction in both directions. It is related to the driver's eye height, object height above the road, distance back from the main road known as the 'x' distance and a distance along the main road known as the 'y' distance. The y distance is related either to the design speed of the road and a corresponding 'stopping sight distance' or in the majority of circumstances is based on observed '85th percentile vehicle speeds'. |
| 'X' Distance | Distance back from the edge of the major road carriageway along the centreline of the minor road used in the measurement of visibility splays. |
| 'Y' Distance | Distance along the nearside edge of the major road from the centreline of the minor road used in the measurement of visibility splays. |

1.0 INTRODUCTION

- 1.1 This publication provides information relating to the Transportation requirements for small development proposals which intend to take access from a rural public road which are in the countryside or settlements outwith the principal towns and villages.
- 1.2 This includes single house applications, applications for 4 or less housing units, and other small mixed-use developments. The document will also be used to assess the suitability of proposed Operating Centres for Heavy Goods Vehicles.
- 1.3 The general principles within this document also apply to larger developments, however early contact with Transportation Officers to confirm requirements relating to a specific larger development is recommended.
- 1.4 This document does not apply to Trunk Roads (A95 and A96) and the Strategic Road Network (A98 and A941).
- 1.5 Throughout this document the term applicant is used, however the information is equally relevant to developers and agents.
- 1.6 All applicants are recommended to seek professional advice to assist with the preparation and submission of a planning application for the development proposal.
- 1.7 A checklist is contained in **Appendix A** to assist all those involved with the preparation of a planning application to meet all of the transportation requirements. The completed checklist should be enclosed with the pre-application enquiry and any subsequent planning application for the attention of Transportation Officers.
- 1.8 The completion of the checklist will help in a number of ways:
 - a) highlight any potential problems for the transportation requirements of the proposed development;
 - b) assist Transportation Officers in processing the planning consultation;
 - c) identify if there is a need for further information or consultation e.g. observed vehicle speed survey data.

- 1.9 For general enquiries and specific development proposals contact Transportation Officers at the address below. A completed Transportation Requirements Checklist (Appendix A) should be submitted with all enquiries.

Transportation Tel 0300 1234565
The Moray Council, Email transport.develop@moray.gov.uk
P.O. BOX 6760,
Elgin,
IV30 9BX

2.0 OBJECTIVES

- 2.1 Moray Council wishes to promote a consistent approach for local developments in rural parts of Moray. This will assist developers and individuals to meet the requirements of the relevant Moray Local Development Plan policies:

- **T2** - Provision of Access;
- **T5** - Parking Standards
- **IMP1** - Developer Requirements
- **IMP3** - Developer Obligations
- **ED7** - Rural Business Proposals
- **H5** - Rural Groupings
- **H7** - Housing in the Countryside

The Moray Local Development Plan 2015 can be viewed at the Moray Council web site.

http://www.moray.gov.uk/moray_standard/page_100458.html

Supplementary Guidance on Housing in the Countryside which supports the Moray Local Development Plan 2015 can be viewed at:

<http://www.moray.gov.uk/downloads/file100517.pdf>

- 2.2 The Main Objectives of this part of the Road Design Procedures and Standards are:-

Safety

- The creation of good access visibility.
- The provision of access lay-bys where appropriate.
- The provision of passing places where appropriate.
- Acceptable spacing from existing junctions and accesses.

Construction

- Standard design approach.
- Acceptable construction standards.
- Compliance with legislation.

Drainage

- Provision of adequate roadside drainage.
- Sustainable drainage system
- Flood risk minimised.

Accessibility

- Support for rural demand responsive transport (DRT)

3.0 SITE ASSESSMENT

3.1 When assessing the location of a new access the applicant must consider the following:-

- Is the proposed access in the best location?
- Can the access be shared with a neighbouring property or development?
- Is the proposed access close to a road junction or other property access?
- Is the proposed access close to a blind bend or crest in the road?
- Can an access visible to other road users be formed?
- Is control over all the land required for any necessary improvements available?
- Is there a need to set back boundary walls or fences?
- Can a suitable gradient for the access road be achieved?
- Do neighbouring properties obstruct visibility?
- Is the site large enough for the provision of car parking and turning?
- Does the existing footpath need to be extended to join with new development?
- Is the development accessed via a single track road without sufficient passing places?
- Does the wider road network leading to the site have any constraints?
- Is the site liable to flooding or are there drainage problems?
- Are there underground services in the footpath or verge?
- Are there overhead services, which could be hazardous?
- Will permission be required to excavate in the road, footpath or verge?
- Are there structures or trees affecting visibility that are listed or are habitats to protected species?
- Is a sheltered storage area for refuse and recycling bins being provided?
- Is there a scheduled bus service within 400 metres of the site?

3.2 The above list is not exhaustive. Guidance is given in the following sections of this publication to assist the applicant to address the above considerations (see **Appendix A Checklist**).

- 3.3 Pre-Application enquiries can be made regarding proposed development through the Development Management preliminary enquiry process:
http://www.moray.gov.uk/moray_standard/page_41735.html

4.0 ACCESSIBILITY – RURAL TRANSPORT

- 4.1 The climate Change (Scotland) Act 2009 sets a target of reducing greenhouse gas emissions by 80% by 2050. New development will have to reduce the need to travel and provide opportunities for travel by public transport. In accordance with Planning Advice Note 75 (PAN75) the intention is for the transport element of new developments to promote genuine choice.
- 4.2 For accessibility walking distances from housing to public transport of 400m to bus services and 800m to rail services are considered reasonable. For many rural areas these distances are exceeded.
- 4.3 Access to public transport is one of a series of aspects considered when new developments are proposed.
- 4.4 The Moray Council has consulted on Supplementary Guidance on Developer Obligations. The funding of bus services, provision of school transport, and public transport priority measures are topics that potentially will be included in the adopted Supplementary Guidance on Developer Obligations.



- 4.5 Applicants will be able to get up to date information and advice on Developer Obligations from the Council web site.

Dial M for Moray – local on demand Bus Service

http://www.moray.gov.uk/moray_standard/page_52932.html

5.0 DESIGN GUIDANCE - RURAL ROADS

5.1 Access Lay-by & Access Layout

- 5.1.1 Across Moray there are a variety of different access layouts onto the public road some of which are historical and do not reflect current design standards. Some of the typical infrastructure which found on single track roads in Moray is summarised at Appendix B. This section of the report details the current design standard requirements for Moray.
- 5.1.2 A new private access, to be constructed on a rural road, will require a combined Lay-by and Access to be incorporated in the layout. Access Lay-bys are required to enable vehicles to keep clear of the road during construction and on completion of the development, delivery vehicles, postal services and refuse vehicles can stop clear of the carriageway without causing an obstruction to the free flow of other vehicles on the public road. The **access lay-by must be constructed prior to commencing work on the development**, as the access lay-by will assist in the safe delivery of building materials to the site, and allow contractors vehicles to manoeuvre and park temporarily. See Drawing shown in **Appendix C** for dimensions and layouts. A Road Opening Permit will be required in order to undertake these works (see 6.2).
- 5.1.3 The access lay-by must have a final layer of bituminous surfacing, to the Moray Council specification, and this must also extend up the private access road for a prescribed distance from the public road edge (see table at 5.3.2). This is to prevent gravel and other material from the private access, being dragged onto the public road. The access lay-by **will not be maintained by the Roads Authority**.
- 5.1.4 The design and construction of a property access and access lay-by must ensure that water cannot run off onto the public road, see Figures 1 and 2 at the end of this section. This is in order to prevent wet areas which could also freeze in winter months.
- 5.1.5 Where a development is below the road level, the drainage system and/or access road should be designed, constructed and maintained thereafter to ensure that water from the road does not affect any properties.
- 5.1.6 Where development is above the level of the road, the access and its drainage system should be designed, constructed and maintained thereafter to ensure that water is not discharged onto the public road.
- 5.1.7 The site may be on a hillside, and in this situation the slope of the first 5 metres of the access from the public road must be no steeper than 1 in 20, and the slope of the access beyond this section should normally be no steeper than 1 in 10.

5.1.8 In the interests of road safety a new access should not normally be situated within 90 metres of a road junction or within 30 metres of an existing property access. If due to the location and layout of the development site this is difficult to achieve then the applicant must contact Transportation Officers for guidance.

ACCESS ARRANGEMENT DRAINAGE PROFILES

ACCESS FALLING FROM A PUBLIC ROAD

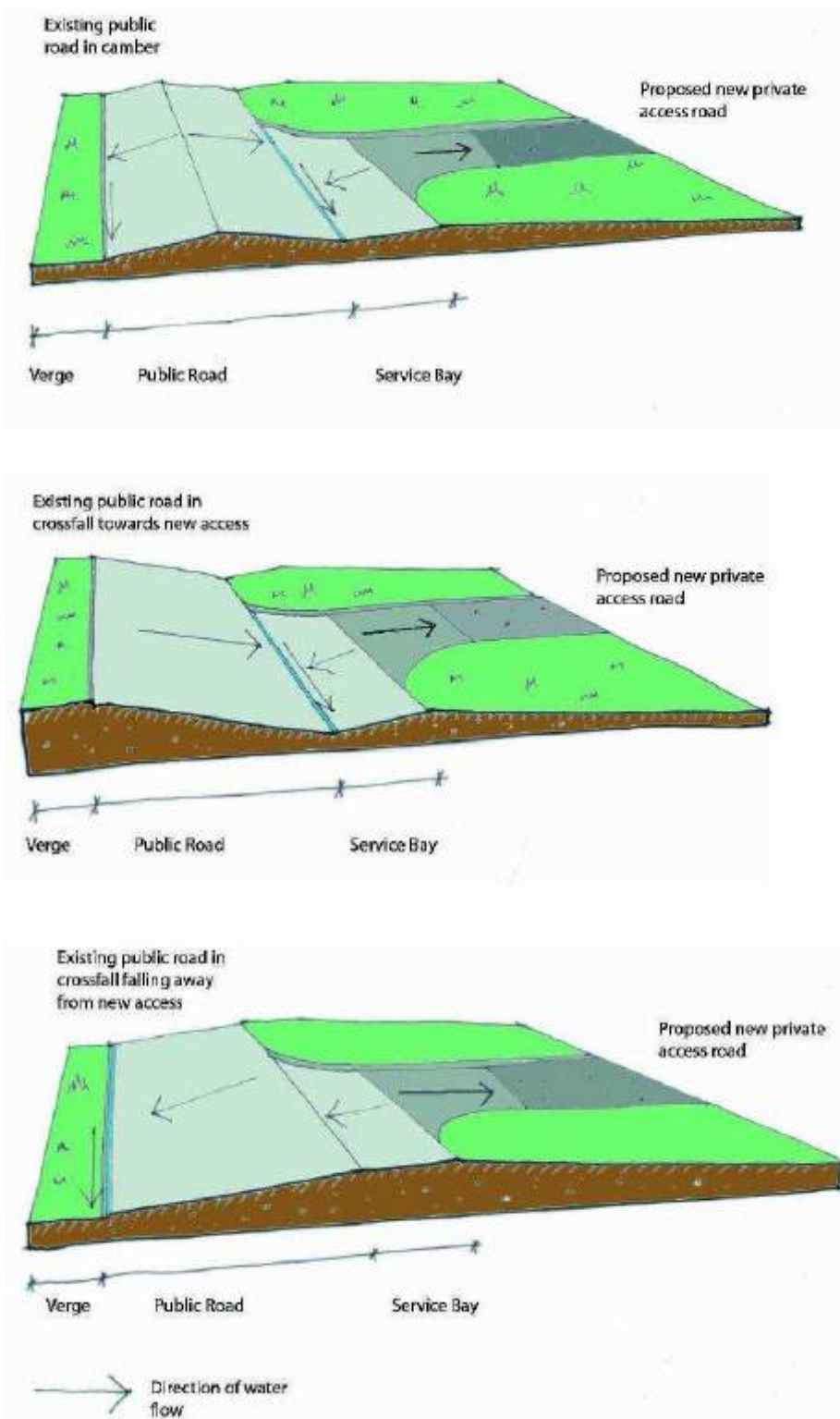


Figure 1: Drainage at access falling away from public road

ACCESS ARRANGEMENT DRAINAGE PROFILES ACCESS RISING FROM A PUBLIC ROAD

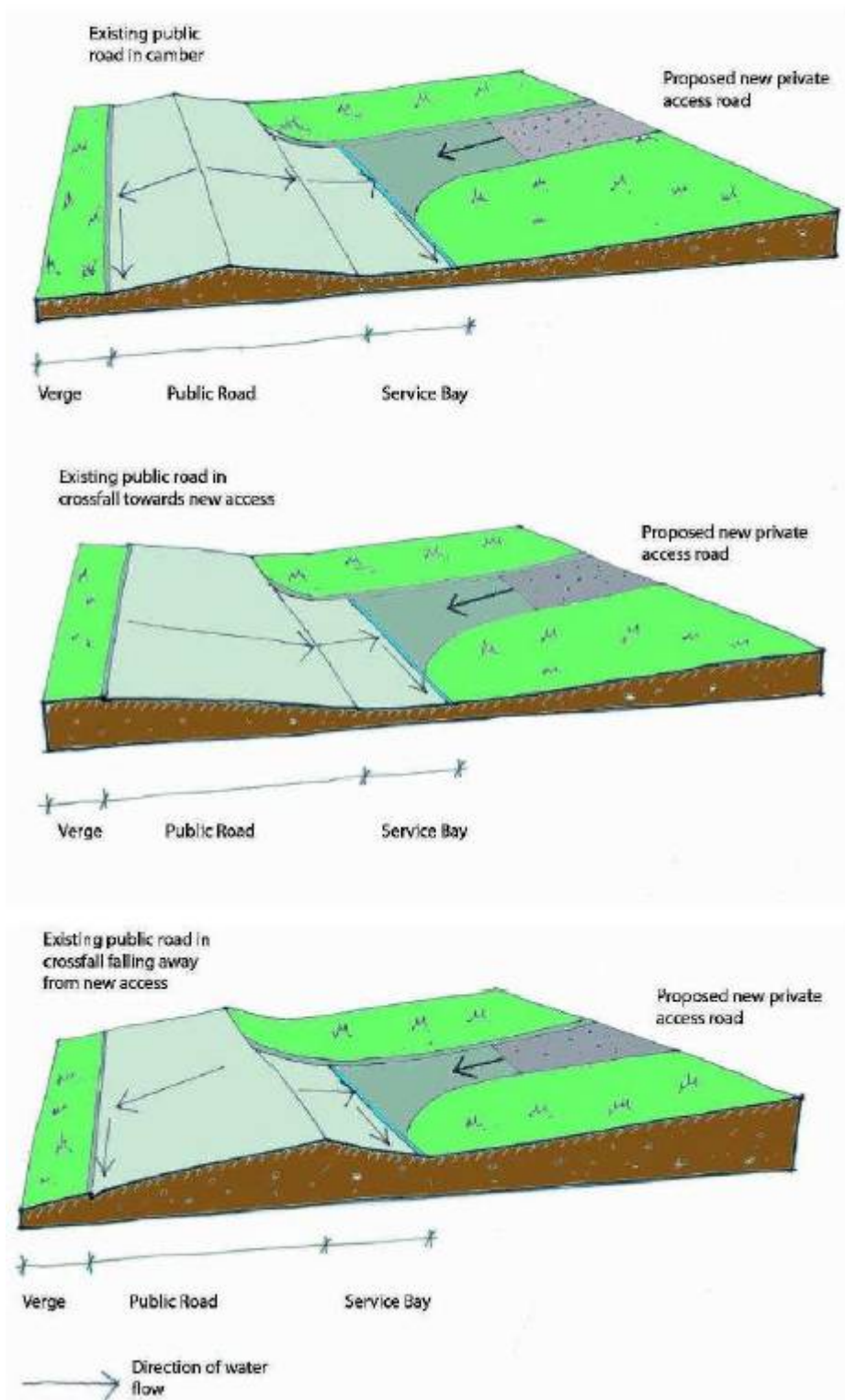


Figure 2: Drainage at access rising from public road

5.2 Access Gates

- 5.2.1 Access gates, if proposed or to be provided, should normally be located at least 8 metres from the adjacent public road edge to ensure vehicles waiting to enter or leave the access do not obstruct the public road while the gates are being opened or closed. Gates must open away from the public road. This distance enables a vehicle with a trailer to stop clear of the public road.
- 5.2.2 Gate pillars, walls, fences or hedges must be kept to the rear of the visibility splay area, or if that is not possible, must not obstruct sight lines (see Section 5.6). Cattle grids if provided must be located beyond the public road boundary.

The Moray Council's supplementary guidance on Housing in the Countryside provides examples of materials and design which are in keeping with the rural character:

<http://www.moray.gov.uk/downloads/file100517.pdf>

5.3 Access to multiple dwellings

- 5.3.1 Where a number of dwellings share a common access there is a need for the access road to be wide enough to enable 2-way traffic for the first section of the access road.
- 5.3.2 The access road must have a final layer of bituminous surfacing, constructed to the Moray Council specification and this must extend up the private access road for a distance of at least 10 metres from the public road edge in order to prevent gravel and other material from the private access, being dragged onto the public road (see table below).

| Summary Table of Access Road Requirements | | | |
|--|---|--------------------------------------|--------------------------|
| | Bituminous surfacing from edge of Public Road | Max Gradient 1 st Section | Width of Access (metres) |
| Single dwelling (adjacent to road) | 5 metres | 1 in 20 | 2.4 – 3.0 |
| Single dwelling Gated Access | 8 metres | | |
| Single dwelling (> 50 metres from road) | 10 metres | | 3.5 |
| Multiple dwellings (> 50 metres from road) | 15 metres | | 5.5 then 3.5 |

5.4 Passing Places on Single Track Roads

- 5.4.1 There are many miles of single track roads in Moray. Many of these do not have an adequate provision of passing places. This often means a deficiency in achieving a suitable and safe access to new developments along these roads.
- 5.4.2 The provision of passing places on single track roads in the countryside requires a strategic approach i.e. passing places which are appropriately spaced and in locations which will provide benefit where there is restricted forward visibility.
- 5.4.3 Proposed new developments that are more than 500 metres from the junction with a S2 (single carriageway 2 lane) road will be required to provide new infrastructure based on the principle of one new passing place on the single-track road per dwelling, where existing provision is deficient.
- 5.4.4 The location of existing passing places on single track roads will be taken into account when considering each development proposal.
- 5.4.5 If an applicant has land which can facilitate a passing place in a strategic location then this method of delivering a passing place will be considered. Any proposal for a passing place should be included in the planning application, shown on the planning application drawings along with evidence of control of the land. Delivery of the passing place would be a condition of the development, prior to the commencement of development. The passing place will form part of the adopted road and will therefore require road construction consent.
- 5.4.6 An assessment of the inter-visibility of passing places will be required to confirm the location and overall number of passing places required between the proposed development and the two-lane road.
- 5.4.7 The dimensions required for passing places are shown at **Appendix C**.

5.5 Wider Road Network

- 5.5.1 Existing constraints on the wider road network will be taken into account when considering small developments in the countryside, in particular where a development is served by a public road which is a no through route. Improvements to public road junctions may be sought, including improvements to sightlines and road width.

5.6 Visibility Splays

- 5.6.1 A well-designed access is important for the safety and convenience of all road users - those proceeding on the public road as well as those using the access. Proposals for a new access or the intensification of use of an existing access will normally have a number of requirements to promote safety and avoid excessive delay
- 5.6.2 Transportation will object to proposals likely to prejudice road safety.
- 5.6.3 Transportation will also raise an objection to the creation of an access and/or visibility splays, unless the applicant is able to demonstrate control or the reasonable prospect of acquiring control of any land likely to be the subject of a condition relating to the provision of any such access and/or visibility splays.
- 5.6.4 Good visibility is essential to enable drivers emerging from the minor road (Private Access/Development Access) to see and be seen by drivers proceeding along the priority road (Public Road)

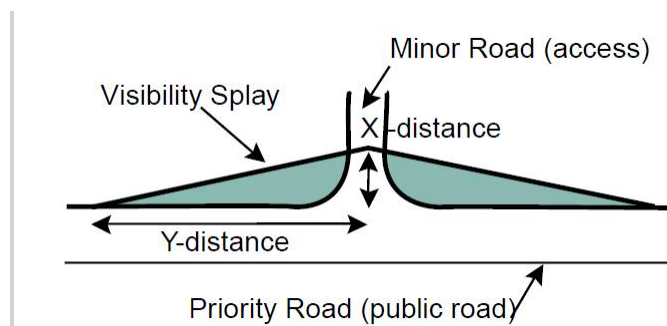


Figure 3: Visibility Splays

- 5.6.5 The x-distance is measured along the centre-line of the minor road from the edge of the running carriageway of the priority road. The y-distance is measured along the near edge of the running carriageway of the priority road from the centre-line of the minor road. Where the access is on the outside of a bend, an additional area will be necessary to provide splays which are tangential to the road edge

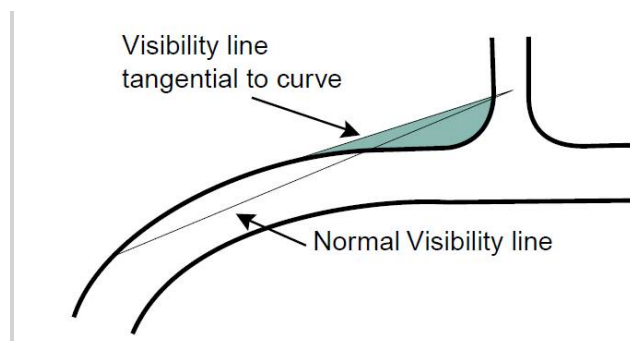


Figure 4: Visibility Splays for Access on Outside of Bend

- 5.6.6 In the case of a new access, x- and y- distances must be adjusted as necessary to allow for any planned road improvements.
- 5.6.7 Forward visibility as shown in Figure 5 is also required to provide inter-visibility between vehicles using the minor road and those proceeding along the priority road. In particular, a vehicle waiting on the priority road to turn right into the access must be able to see oncoming traffic and be seen by following traffic. Forward visibility depends on the same factors as y-distance.

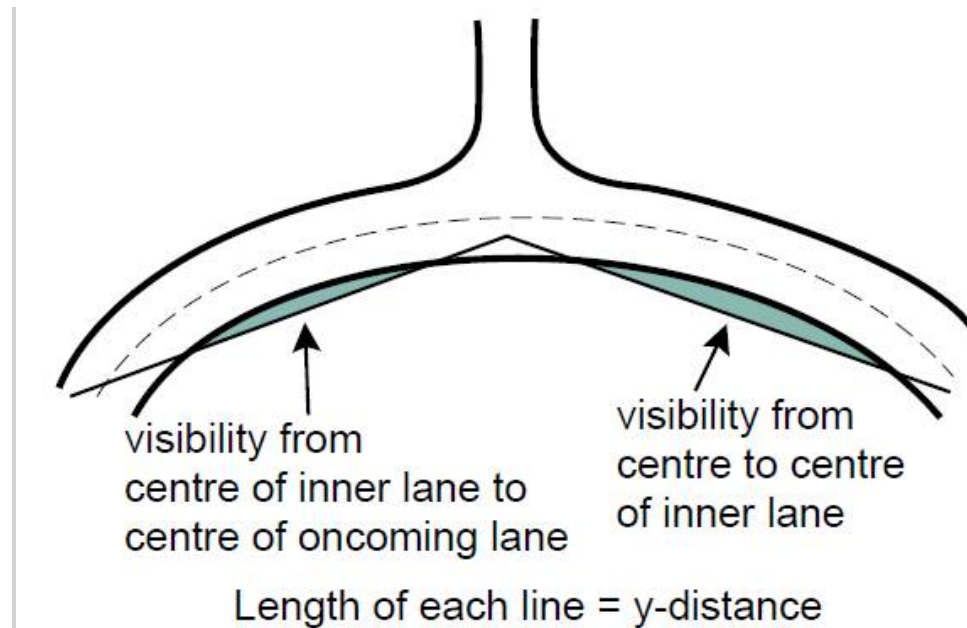


Figure 5: Forward Visibility Requirements

- 5.6.8 The size of the visibility splay depends on the speed limit or observed vehicle speeds on the public road. It is necessary to consider the driver's line of vision, in both the horizontal and vertical planes, and the stopping distance of the vehicle. Where the applicant does not provide observed vehicle speed data, the speed limit will normally be used.
- 5.6.9 The distance along the public road, Y distance, is the distance the driver needs to see along the road edge (see table below). This is measured from the centre line of the access to the location on the road of the approaching vehicle, which varies depending on the speed of approaching traffic. The faster the approaching vehicles, the longer the distance required to see and be seen.
- 5.6.10 The distance back from the public road, X distance, is shown in the table below. The distance varies according to the number of dwellings. The distance is taken from the edge of the carriageway back along the centre line of the private access.

5.6.11 The visibility splay must be assessed between minimum driver's eye line 1.05 metres above the road up to a height 2m above the road and to objective points at the end of the Y distance normally between 0.6m and 2m above the carriageway surface. The assessment must consider obstructions to visibility within the visibility splay including the horizontal and vertical topography in between i.e. hidden dips and crests along the road and any large utilities or other infrastructure already in the area between these points.

5.6.12 For situations with more complex circumstances, such as vertical and horizontal alignment issues, a detailed topographical survey may be requested to ensure the vertical and horizontal zones can be clearly ascertained and any necessary modifications identified.

5.6.13 The following table shows the Y and X values based on speed limit values.

| | | | | |
|---------------------|---|-----|-----|-----|
| Speed Limit | 30 | 40 | 50 | 60 |
| Y Distance (metres) | 90 | 120 | 160 | 215 |
| X Distance (metres) | Single dwelling = 2.4m; > 1 dwelling = 4.5m | | | |

5.6.14 The y values shown are based on the speed limit of the public road. It may be possible to reduce y values if actual traffic speeds are provided. This should be based on survey data gathered over a minimum duration of one week at locations to be agreed with the Moray Council.

5.6.15 The access, lay-by and visibility splay (both those beside the minor road and those required for forward visibility) must be established before building work commences, to ensure a safe access for builders and tradesmen.

5.7 Providing and Maintaining Visibility Splays

5.7.1 When submitting a planning application it is necessary for the applicant to demonstrate that they have, and can maintain control over the visibility splay area. The applicant will have responsibility for the maintenance of unobstructed sight lines over the visibility splay area. If the visibility splay area includes any neighbouring land then the applicant will need to discuss this with the landowner and make arrangements to satisfy the requirement to demonstrate adequate control for the lifetime of the development.

- 5.7.2 Applicants should give careful consideration to the Trees and Development Supplementary Guidance. Applicants should note that there is a presumption against the felling/removal of trees purely to form an access/visibility. For the avoidance of doubt the visibility splay is an essential feature required for achieving the Moray Local Development Plan Policy T2 Provision of Road Access.
<http://www.moray.gov.uk/downloads/file100519.pdf>
- 5.7.3 There may be circumstances when the developer wishes to locate the private access on or near a bend in the road. The outside of a bend is the safest option. The extent of the visibility splay must be clearly identified.
- 5.7.4 If there is no alternative arrangement other than to locate the access on the inside of a bend, the applicant must be fully aware of the extent of the area which will be affected by the visibility requirements which they must demonstrate that they have, and can maintain control over, and which must be kept free of obstructions such as buildings, trees shrubs and long grass or other vegetation. In these circumstances early consultation with Transportation officers is recommended.
- 5.7.5 Once provided, visibility splays must be retained and kept clear. In this regard it will be helpful for trees and shrubs to be planted at least 3m to the rear of the visibility splay to allow for future growth.
- 5.7.6 Any boundary walls/fences must be set back to a position behind the required visibility splays.
- 5.7.7 To reduce the impact of an access on the countryside, its location and design must be carefully considered and existing access, including lanes, should be used where possible.
- 5.7.8 Transportation will not introduce a speed limit or warning signage simply to facilitate a new access.
- 5.7.9 Reductions in visibility standards will not be permitted simply because the applicant does not control the required visibility area or does not have a reasonable prospect of bringing it under his control
- 5.7.10 If a dwelling access is located near a junction, visitors might park their vehicles on the priority road and obstruct junction visibility. To reduce this risk, dwelling accesses should not normally join a priority road within the y-distance of a junction.

5.8 Parking and Turning of Vehicles

5.8.1 The provision of the minimum number of parking spaces within the curtilage of the property, based on the number of bedrooms, is the normal requirement for rural developments. The levels of parking required are shown in the following table.

| Dwelling Description | Spaces |
|----------------------|--------|
| 3 or fewer bedrooms | 2 |
| 4 or more bedrooms | 3 |

5.7.2 An equally important requirement is the provision of an adequate turning area. Vehicles that enter the development must be able to turn round out with designated parking spaces, and leave the site in forward gear.

5.7.3 This requirement applies, whether you are developing a single house site, or a site with several properties. Vehicles must not reverse out onto the public road. Applicants should consider how materials such as heating oil are delivered and for example place their fuel tank where it can be accessed from the access lay-by. Where there is more than one house this may not be a practical option, then sufficient turning area would normally be required within the development site to enable the delivery vehicle to turn round. Areas provided for turning are in addition to those provided for parking.

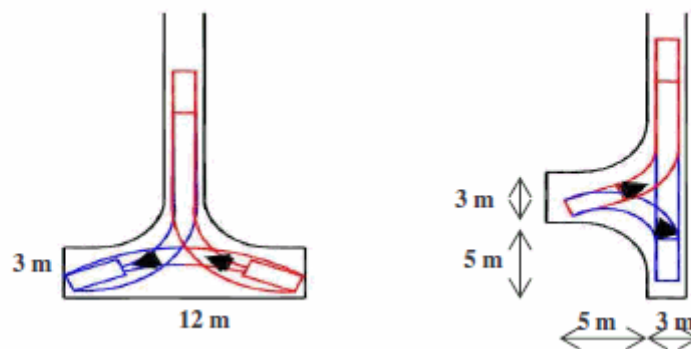


Figure 6: Turning Area Geometry

5.7.4 Many areas of Moray can experience severe winter weather and for those developments where the access road is particularly steep (a gradient steeper than 1 in 10), consideration should be given to providing, where land is available, parking areas at the top/bottom of the incline.

5.9 Roadside Drainage

5.9.1 Any existing roadside ditch should be taken, where possible, to the rear of the access lay-by and under the access by a pipe or culvert. Watercourses within the site, which are running alongside the public road at a point upstream or downstream on the site, must be accommodated, including piping under any access track. The applicant should inspect adjacent pipes or culverts to ascertain the size of pipes that have been used downstream of the location for the proposed access to assist in determining the size of pipe. This is deemed necessary to ensure the appropriate diameter of pipe identified by local landowners, with previous knowledge of flood conditions, is used. However it must not be assumed that the size of existing adjacent pipes is acceptable for the pipe under the new access track. **The applicant must consult Transportation Officers to confirm the acceptable diameter of pipe to be used** which must be a minimum of 300 mm diameter. Locations with deep and wide ditches will require larger diameter pipes. As a general rule, the bigger the pipe, the lower the risk of blockage, so the principle is to go up a size rather than down. All pipes should be laid at a self-cleansing gradient. The table below shows the typical gradients that should be used.

| Pipe Diameter (mm) | Self Cleansing (minimum) gradient |
|--------------------|-----------------------------------|
| 300 | 1 in 150 |
| 375 | 1 in 188 |
| 450 | 1 in 225 |

5.9.2 Prior to laying the pipe, the ditch should be cleaned out along the frontage as necessary, the pipe placed on, and surrounded by concrete (150mm thick).

5.9.3 Headwalls of masonry or concrete should be provided, so the end of the pipe can be located, should the pipe become blocked during flood conditions. Standard details can be provided upon request to (transport.develop@moray.gov.uk)

5.9.4 The final bitumen macadam surface of the access lay-by must be shaped to prevent water, from the new access, flowing onto the public road. This is to prevent wet areas, which could freeze in winter. The developer should also take steps to ensure water flowing along the public road does not, and cannot enter the site, and give rise to flooding problems.

5.9.5 On many rural roads, there are no ditches and roadside drainage is by offlets or grips, (small ditches cut into and across a verge) which discharge water into adjacent land. The applicant must inspect the site, and where offlets currently discharge into the site, must accept that water and make provision to deal with, and dispose of it.

5.10 Dealing with Storm Water

5.10.1 During heavy rain, water from agricultural fields can flow onto the road, culverts become choked, and roadside ditches overtop with the result that significant water flows along the public road. The applicant should take measures to prevent water flowing into their development. The simplest solution to deal with this water is to ensure that the access to the property slopes upwards for a short distance, forcing the flood water to continue along the road edge, until it finds its natural escape route.

5.10.2 Where water can enter the site, ditches or a piped drainage system should be installed within the site and water re-directed back into the natural drainage system.

5.11 Potential Flooding Risk Factors

5.11.1 In order to avoid building on sites susceptible to flooding, the following risk factors should be considered.

- Is there a roadside ditch with significant water flow or signs of flow;
- Are there adjacent streams, burns or ditches which could overtop in storm conditions;
- Does the Topography of the site form a natural basin, in which water could accumulate?
- Are there any underground springs discharging water on the site?
- Any natural gullies which could direct water run-off towards the site?
- Higher ground above/behind that may require the installation of cut off drains?
- The proximity of the site to a river (erosion and/or flooding).
- The height of a shoreline site in relation to High Tide Level (Mean High Water Spring - normally the highest level to which spring tides reach on the average over a period of time).

5.11.2 The applicant should examine the SEPA Flood Map at:

www.sepa.org.uk/flooding

However it should be noted that this relates to large-scale flood risk assessment and does not show sites that may be susceptible to localised flooding. Additional information on local flooding may also be obtained from Moray Flood Risk Management and Roads Maintenance.

Flood Risk Management, Consultancy, Moray Council IV30 1BX Tel 01343 543451

http://www.moray.gov.uk/moray_standard/page_80332.html

Roads Maintenance: PO Box 6760, Elgin, IV30 19BX (0300 123 4565)

http://www.moray.gov.uk/moray_standard/page_40815.html

5.11.3 Sites that may be at risk of flooding will require a flood risk assessment to be undertaken by a suitable specialist.

6.0 LEGAL RESPONSIBILITIES AND REQUIREMENTS

6.1 Introduction

6.1.1 Obtaining Planning Permission for a new development is the first step in the legislative process. Proposals for development including changes to submissions should be discussed with the Councils Development Management Service.

http://www.moray.gov.uk/moray_standard/page_41669.html

6.1.2 Further permissions are required under different and separate legislation, including Roads and Building Control.

6.1.3 The requirements of Roads legislation, to enable an applicant to create a private access from a public road or to adjust the existing access to a site, is set out below in sections 6.2 and 6.3.

6.1.4 Certificated contractors, whose employees have the necessary RAUC (Road and Utilities Committee) certificates, should carry out work on or adjacent to the public road. Where the road, verge, or footpath contains apparatus from suppliers such as telecom, gas, water, and electricity (called statutory undertakers) these services must be protected, and RAUC approved personnel must be employed to carry out the work. The statutory undertakers (by legislation) require to be notified of any proposed excavation works via the Scottish Road Works Register (see 6.2 for information).

6.1.5 The applicant or the appointed contractor must obtain the appropriate permissions from the Council and provide the necessary information to enable the Council to input details of the proposed works on the Scottish Road Works Register.

6.1.6 When requesting connections to Gas, Electricity, and Telecom, these undertakers usually employ their own contractors, and will complete the required notices direct, but for Water & Sewerage connections you or your contractor are normally responsible for obtaining the appropriate consents from the Council and Scottish Water.

6.1.7 In order to ensure the applicant and the contractor are familiar with the up to date procedures, these are outlined in the following sections. Permits and further details are available from Transportation Officers.

6.2 Road Opening Permits

6.2.1 A road opening permit is required, if you excavate in the road, verge, or footpath, for a new access, a water or sewerage connection, or intend to lay a buried cable or pipe across or along the public road. This is a requirement of section 109 of the New Roads & Street Works Act 1991 and/or section 56 of the Roads (Scotland) Act 1984. Statutory undertakers require a minimum of 10 working days notice of the intended works, therefore you should request a Permit from the Council, in advance of your proposed start date so that the Council can input the information on the Scottish Roadwork Register. The recommended minimum notification period for standard works is 15 working days. For details of the notification requirements for other works including minor and major works and fee for the permit and subsequent inspections, see the web page link provided at 6.3.2.

6.3 Occupation of the Road

6.3.1 The Roads (Scotland) Act 1984 requires that permits be obtained for the following activities.

- To deposit a skip on the road.(Section 85)
- To deposit building materials on the road, footway, or verge. (Section 58)
- To erect scaffolding on the road footway or verge. (Section 58)
- To enable a crane to work on the road. (Section 58)

6.3.2 Normally the contractor will apply for these permits from the Council and pay the appropriate fees, but there may be circumstances, depending on how the work is managed, that the applicant is obliged to apply for the permits. Remember to check, as you don't want your work stopped or delayed for lack of permits. For Permits to undertake all of the above work, or for further advice, the applicant should contact Transportation Officers.

Transportation Manager
The Moray Council,
P.O. BOX 6760,
Elgin,
IV30 9BX

Tel 0300 1234565
Email roadspermits@moray.gov.uk

To download the most up-to-date Application for Road Opening Permit go to the following web page:

http://www.moray.gov.uk/moray_standard/page_57699.html

7.0 CONSTRUCTION

7.1 Signing & Safety

7.1.1 Once information is obtained from the Statutory Undertakers on the location of their apparatus and the appropriate permit from the Roads Authority has been received the applicant/contractor is able to proceed with the excavation of the road, verge or footpath. The applicant/contractor must ensure that the works are carried out in a safe manner, correctly signed and guarded to ensure the safety of the workers at the site and members of the public passing the works.

7.1.2 The applicant/contractor must be familiar with the Code of Practice “Safety at Street Works and Road Works” issued by the Scottish Government and others which is available via the web link below. Failure to comply with the code of practice may lead to criminal prosecution in addition to any civil proceedings.

https://www.gov.uk/government/uploads/system/uploads/attachment_data/file/321056/safety-at-streetworks.pdf

**ROAD DESIGN PROCEDURES AND STANDARDS
APPENDIX A – TRANSPORTATION REQUIREMENTS CHECKLIST**

Providing a completed checklist to Transportation Officers will identify if there is a need for further information, potential problems and assist with processing the planning consultation.

SITE ASSESSMENT

Is there a Previous Planning Application for this location? Yes No

If YES give the application reference number if known.

National Grid Ref at the Access Point to the Public Road

Is there a new private access from the Public Road? Yes No

Is there an enlargement of an existing private access? Yes No

Does it involve sharing an existing private access? Yes No

How many properties will the access serve?

ROAD SAFETY

Is the Private Access located on a class A or B road? Yes No

Is the Private Access onto a single track with passing places? Yes No

If YES what is the distance to the nearest two lane road and how many Passing Places (only those designated with signs) are there between the access and the road. _____m _____no.

Road / street name or location

What Speed Limit applies to this Road / Street?

The observed traffic speed at the access in MPH (attach a copy of survey data)

Visibility Splay Length to be provided in Metres.

Left

Right

How close to the nearest Road Junction is the proposed access?

Is the area of land within the Visibility Splays in the Ownership or control of the applicant?

Yes

No

Do walls or fences require to be set back?

Yes

No

Does the new access cross a Cycleway?

Yes

No

Are you constructing a new Access Lay-by?

Yes

No

Are you utilising an existing Access Lay-by?

Yes

No

DRAINAGE

Have you inspected the existing Roadside Drainage?

Yes

No

Is a new Pipe Culvert required?

Yes

No

If yes – what diameter of pipe culvert is proposed?

Are any major ditches or watercourses near or on ground above the site?

Yes

No

Has the SEPA Flooding Database been examined

www.sepa.org.uk/flooding

Yes

No

Is the site considered to have any Flood Risk?

Yes

No

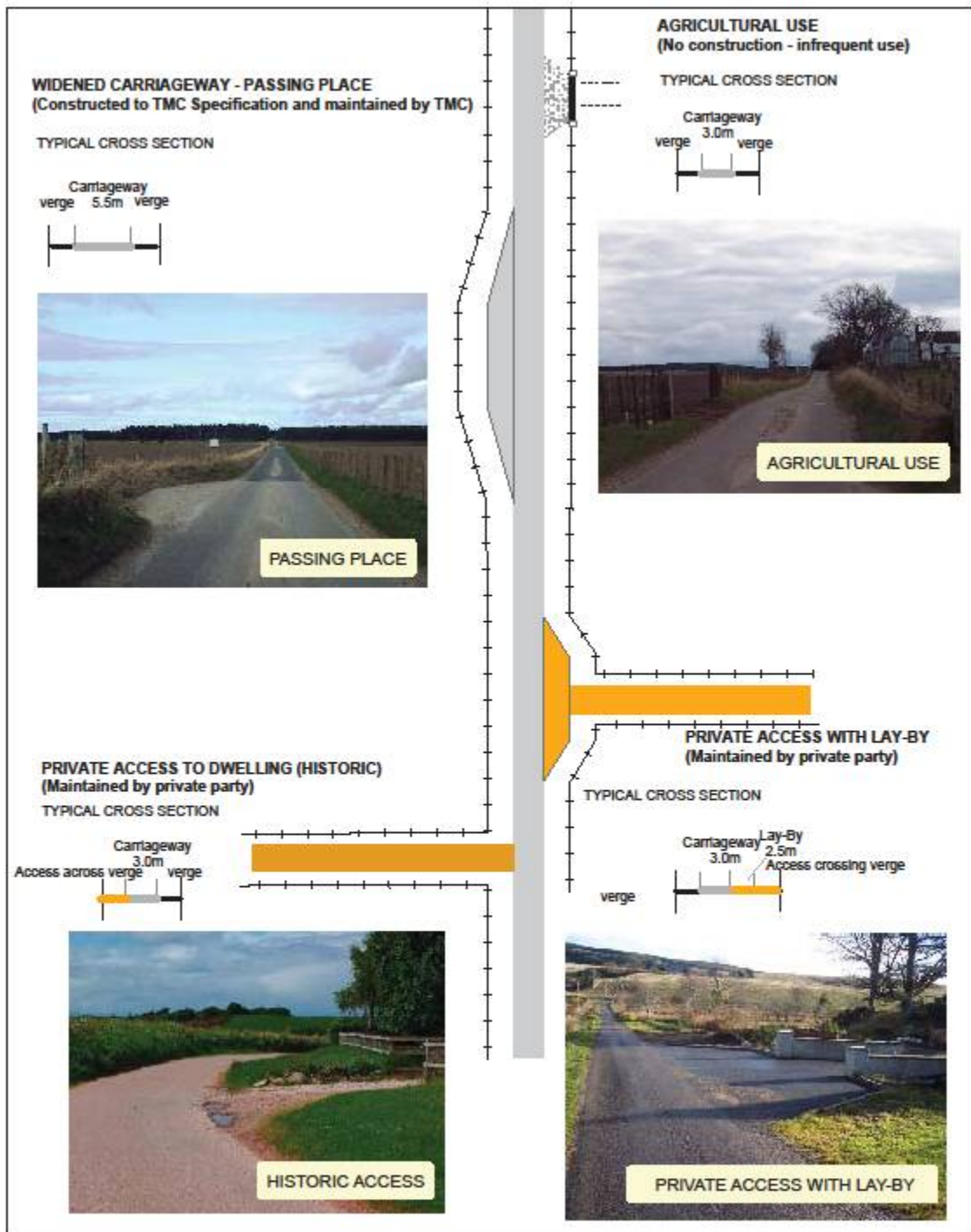
Has a competent person undertaken a flood risk assessment?

Yes

No

ROAD DESIGN PROCEDURES AND STANDARDS

SINGLE TRACK ROADS – TYPICAL INFRASTRUCTURE



Single Track Roads - Typical Infrastructure

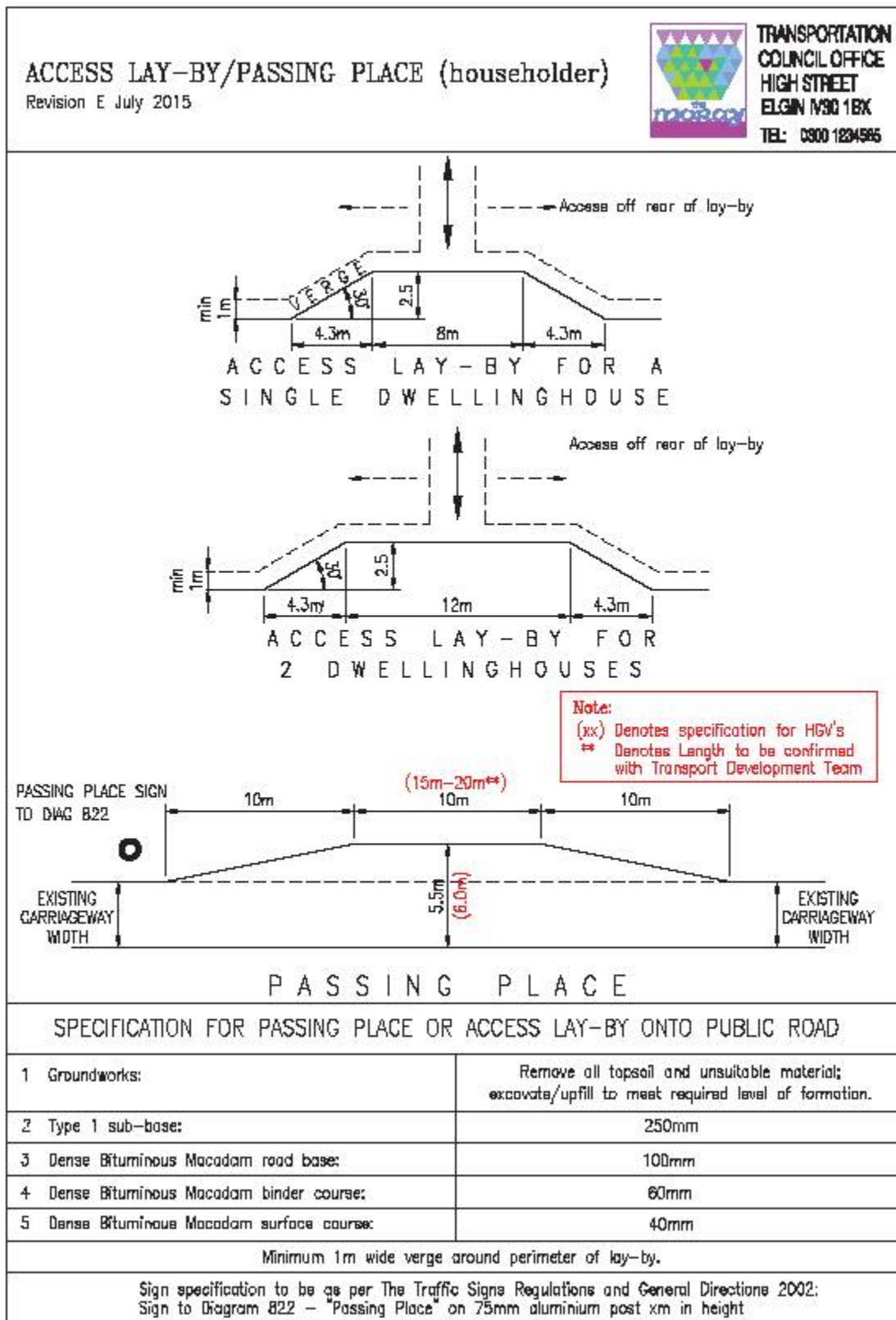
Notes : Carriageway width at Passing Place = 5.5m

GREY - Denotes maintained by The Moray Council
ORANGE - Denotes Maintained by private party

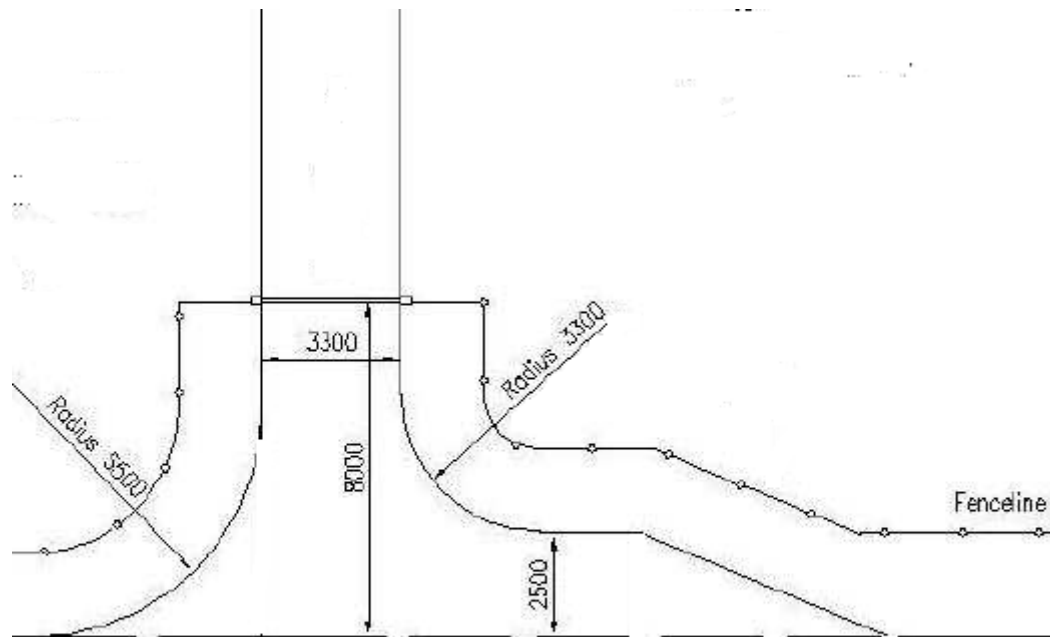


ROAD DESIGN PROCEDURES AND STANDARDS

APPENDIX C – ACCESS LAY-BY AND PASSING PLACE SPECIFICATION



Access lay-bys can be less angular than shown above provided the basic dimensions are complied with, for example:



These features can be softened by appropriate planting provided these don't interfere with visibility splays.

List of Amendments (08/03/2016)

Glossary: add reference to National Roads Development Guide, add reference to Moray Local Development Plan 2015, rewording of some definitions.

Para 1.2: include Operating Centres for Heavy Goods Vehicles within scope of requirements.

Para 1.4: add roads excluded from scope of requirements.

Para 1.10: add requirement for completed checklist to be submitted with enquiries.

Para 2.1: update list of related Moray Local Plan policies.

Section 4: updated advice.

Para 5.1.5: introduces specific requirement not to discharge water onto public road.

Figures 1 & 2: typical sections relating to drainage at junction.

Para 5.2.2: reference to guidance on Planning in the Countryside.

Section 5.5: advice that existing constraints on the wider road network will be taken into consideration.

Section 5.6: completely replaced.

Section 5.7: added further guidance on providing and maintaining visibility splays.

Appendix C additional layout for access lay-bys

Other rewording to improve clarity.

Amendments following Consultation

Glossary: amend description of Roads Authority to 'roads authority for local public roads only'

Para. 1.6: removed

Para 2.1: additional reference to Supplementary Guidance on Housing in the Countryside

Para. 5.8.1 correct typo in table relating to number of bedrooms

Para 5.9.1 additional text to clarify that adjacent pipe size may not be acceptable for pipes under new accesses.

Para 5.11.2 update reference to Moray Flood Risk Management

Para 6.1.14 replace 'HAUC' with 'RAUC' and 'Highways' with 'Road'.
Renumbering of figures and paragraphs